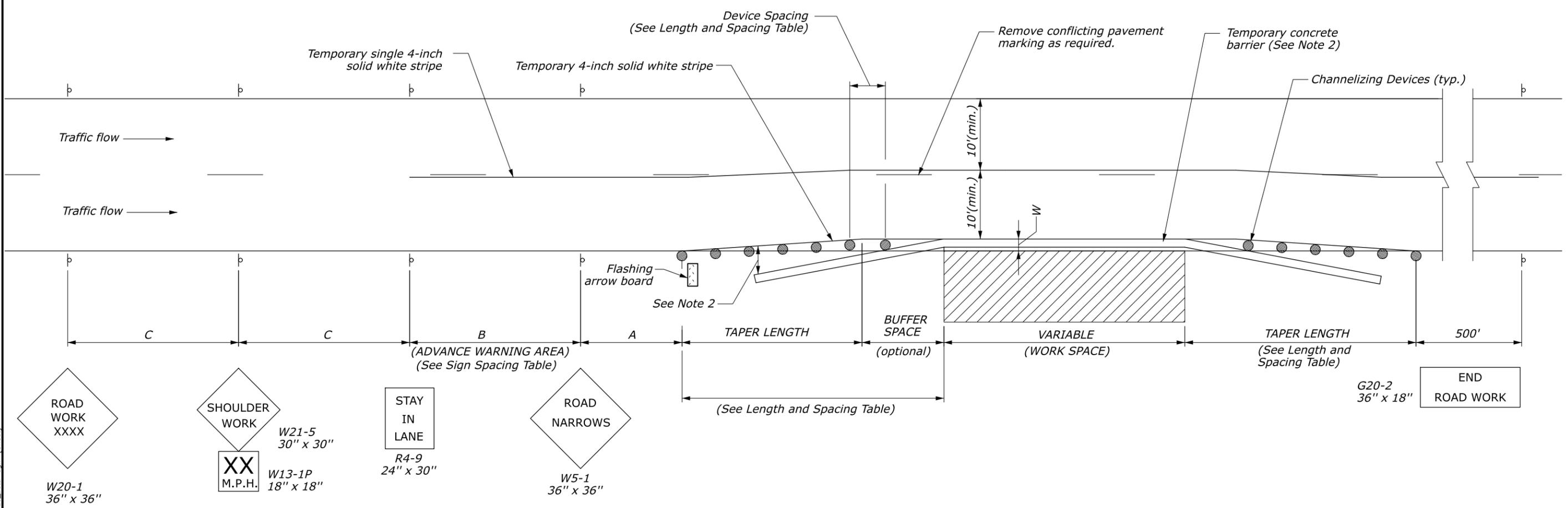


TYPICAL APPLICATION: ONE WAY, TWO LANE ROADWAY, CLOSING PARTIAL LANE AND SHOULDER USING TEMPORARY BARRIER



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NOTES:

- Final location and spacing of temporary traffic control devices may be changed to fit field conditions as approved by the CO.
- Place barrier according to the AASHTO Roadside Design Guide, latest edition. Terminate barrier ends outside the clearzone or protect the ends of barrier with a crash cushion. Mount reflectors on barriers at 25-foot intervals.
- Repeat signs on both sides of the roadway.

APPROACH SPEED* (MPH)	MINIMUM TAPER LENGTH (LNFT)	BUFFER SPACE LENGTH (LNFT)	CHANNELIZING DEVICE SPACING (LNFT)		
			TAPER AREA	BUFFER SPACE	WORK SPACE
20	Shifting taper formula:	115	20	40	40
25	$L = \frac{WS^2}{120}$ for $S \leq 40$ MPH	155	25	50	50
30		200	30	60	60
35	$L = \frac{WS}{2}$ for $S \geq 45$ MPH	250	35	70	70
40		305	40	80	80
45	Where:	360	45	90	90
50	L = Minimum length of taper	425	50	100	100
55	W = Width of offset in feet	495	55	110	110
60	S = Numerical value of posted speed limit or 85 percentile speed prior to work in miles per hour	570	60	120	120
65		645	65	130	130
70		730	70	140	140

* Approach speed based on the regulatory posted speed, not the advisory speed.

ROAD TYPE	DISTANCE BETWEEN SIGNS (LNFT)		
	(A)	(B)	(C)
Urban and Rural 30 MPH and less	100	100	100
Urban and Rural 35 MPH to 50 MPH	350	350	350
Rural greater than 50 MPH	500	500	500
Expressway / Freeway	1000	1500	2640

NO SCALE

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION OFFICE OF FEDERAL LANDS HIGHWAY	
EFLHD DETAIL	
TEMPORARY TRAFFIC CONTROL ONE WAY, TWO LANE ROAD SHOULDER WORK WITH MINOR ENCHROACHMENT (WITH BARRIER)	
DETAIL APPROVED FOR USE	DETAIL
APPROVED: MAY 2011	E635-10
REVISED: SEPTEMBER 2020	